



CITY OF LUFKIN
 ENGINEERING DEPARTMENT
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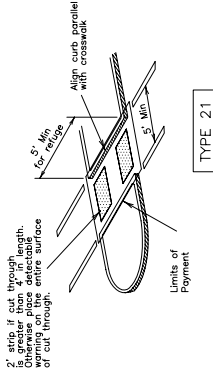
STREET STANDARDS

HANDICAP RAMP DETAILS

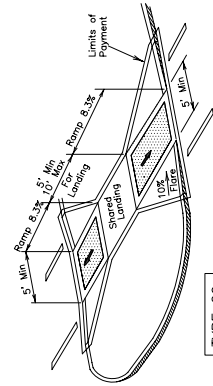
02

02-04-04

02_HDRAMP.DWG



TYPE 21



TYPE 20

CURB RAMPS AT MEDIAN ISLANDS

GENERAL NOTES

All slopes are maximum allowable. The least possible slope that will still drain properly should be used. Ramp length or grade of approach sidewalks may be adjusted as directed by the Engineer.

The minimum sidewalk width is 5'. Where a 5' sidewalk can not be provided due to site constraints, a minimum 3' sidewalk with 5' x 5' passing areas at intervals not to exceed 200 ft is required.

Landings shall be 5' x 5' minimum with a maximum 2% slope in any direction. Maneuvering space at the bottom of curb ramps shall be a minimum of 4' x 4' minimum on the crosswalk and wholly outside the parallel vehicular travel path.

Maximum allowable cross slope on sidewalk and ramp surfaces is 2%.

Curb ramps with returned curbs may be used only where pedestrians would not normally walk across the ramp. Otherwise, flared sides shall be provided.

All concrete surfaces shall receive a light broom finish unless noted otherwise in the plans.

Landing textures shall consist of truncated domed surfaces. Textures are required to be detectable underfoot. Surfaces that would allow water to accumulate are prohibited.

Ramp textures shall have grooves 1/4" deep and 3/4" wide 2' apart and arranged so that water will not accumulate. Other designs for surfaces that are detectable underfoot may be submitted for consideration. Additional information on curb ramp location, design, light reflective value and materials may be found in the current edition of the Texas Accessibility Standards (TAS) promulgated and administered by the Texas Department of Licensing and Regulation (TDLR).

Raised medians separate opposing directions of traffic, and provide a refuge area for pedestrians unable to cross the entire roadway in the allotted signal phase. To serve as a refuge area, the median should be a minimum of 5' wide. Medians should be designed to provide accessible passage over or through them.

Small channelization islands, which can not provide a minimum 5' x 5' area, shall be cut through level with the surface of the street.

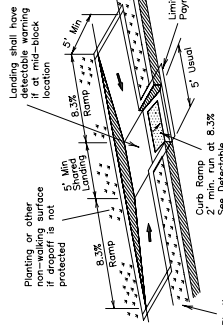
Crosswalk dimensions, crosswalk markings and stop bar locations shall be as shown on the plans. Where crosswalk markings are not required, markings are not required, ramps shall be aligned with theoretical crosswalks, or as directed by the Engineer.

Existing features that comply with TAS may remain in place unless otherwise shown on the plans.

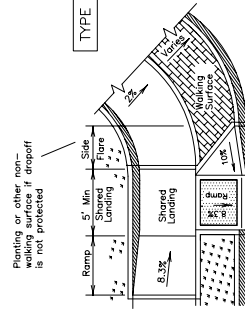
Headcills are not required on curb ramps. Curb ramps shall be provided wherever an accessible route crosses (penetrates) a curb.

Shaded areas indicate locations of detectable warnings. (Color / light reflective value and texture contrast)

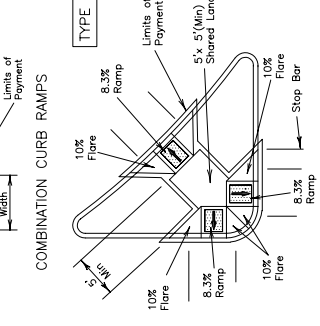
Curb ramps and landings shall be constructed and paid for in accordance with the current edition of the Texas Accessibility Standards. The cost of the ramp will be paid for in accordance with Item "Concrete Curb, Gutter and Combined Curb and Gutter".



TYPE 3

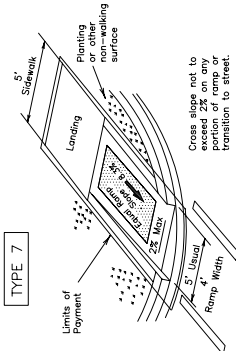


TYPE 6



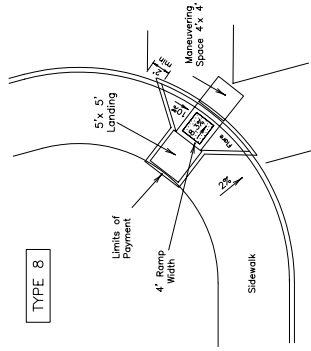
TYPE 22

COMBINATION ISLAND RAMPS



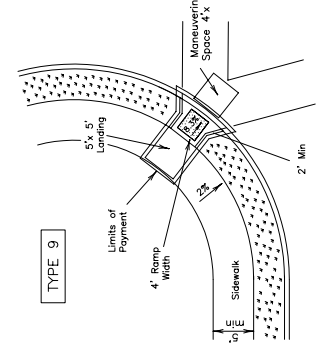
TYPE 1

DIRECTIONAL RAMP WITHIN RADIUS



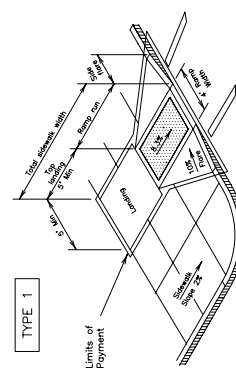
TYPE 8

DIAGONAL CURB RAMP (FLARED SIDES)



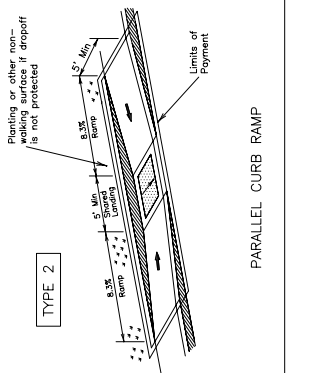
TYPE 9

DIAGONAL CURB RAMP (RETURNED CURB)



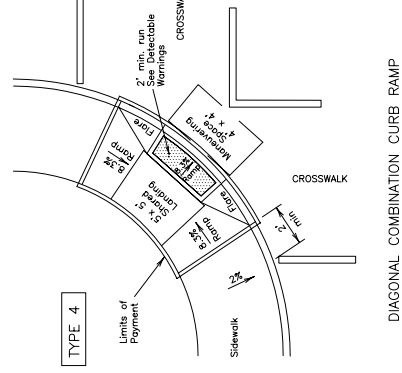
TYPE 2

PERPENDICULAR CURB RAMP



TYPE 4

PARALLEL CURB RAMP



TYPE 4

DIAGONAL COMBINATION CURB RAMP
 Perpendicular to the tangent of the Curb
 Radius and Contained in Crosswalk